

From First Siren to the Last Train: Haydarpasha Train Station

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Abstract

In 149 year period from 1870 Ottoman epoch to modern day, actualization of railway projects starting with Haydarpasha-Hejaz Railway put forward the necessitation of building facility together with railhead and stations. Despite these investments placed huge burdens to the treasury at painful last era of Ottoman Empire, it was decided from necessity to build a new railhead building. At the same time, by putting out the project designing of this magnificent construction to show the power of Sultan Abdulhamit the Second to tender and giving the work to German Phillip Holzmann & Co. Company, the construction of the new Haydarpasha Railhead was realized. During the completion of the Railhead, after the 1909 Fire and 1917 Explosion, the Railhead was hardly restored between the years 1930-1933. The Railhead exposing to several physical and climatic effects was taken to repairmen in 1976 and 1983. Haydarpasha Railhead has experienced the biggest damage at the explosion of Independenta Oil Tanker having Romania flag. During the continuing explosions after its grounding in front of Haydarpasha, colored stained-glasses of the railhead were damaged in a great extent. A large part of the roof was burnt inadvertently during the repairmen on 15 November 2010. With surveying and restoration project applied in 2014, a new renovation period at the whole of the building was started.

Key Words: Haydarpasha Railhead, Otto Ritter, Philipp Holzmann

1. Introduction

By the opening of Haydarpasha-Izmit railroad on 3 May 1873, the first railway station was built as a small structure at the area where Haydarpasha Bridge stands today. 33 years after that day, the construction of project of today's structure that was prepared by two German Architect Otto Ritter and Helmuth Cuno was started on 30 May 1906 by German Philipp Holzmann, and was put to service after completion on 19 August 1908. Since that day, during its 115 year history people borne witness, the rich and the poor meet on wooden banks (Binark, Kocabiyik, (2007)), Haydarpasha Railhead has become a symbol of the coming and going routing of Anatolian people in the movies and in the poem of Nazım Hikmet:

Human Landscapes from my Country

At Haydarpasha Railhead

In 1941 spring

The hour fifteen

Stairs under the sunshine

exhaustion and rush

A man standing,

Thinking somethings...

....

By the start of Marmaray construction in 2012 to make a nonstop travelling, commuter trains of Halkali-Sirkeci and Haydarpasha-Pendik were stopped, and the public opinion has been tried to be understood by various alternative projects for 115 year old Haydarpasha Railhead and its environment.

2. The First Railhead, the First Siren

Haydarpasha situated between Kadikoy and Uskudar districts is named after Haydar Pasha who was grown up in architects' guild during Bayezid the Second and Suleyman the Magnificent era, who was the governor of Sivas and Algeria, and after duty of council vizier his name was given to himself after the vineyards gifted to him as a response of Kavak Mosque Sultan Suleyman the Magnificent commissioned in Uskudar [Url 1]. Evliya Celebi's making reference to the vineyards of Haydar Pasha strength the idea tha the area was firstly named after Haydar Pasha (Figure1).

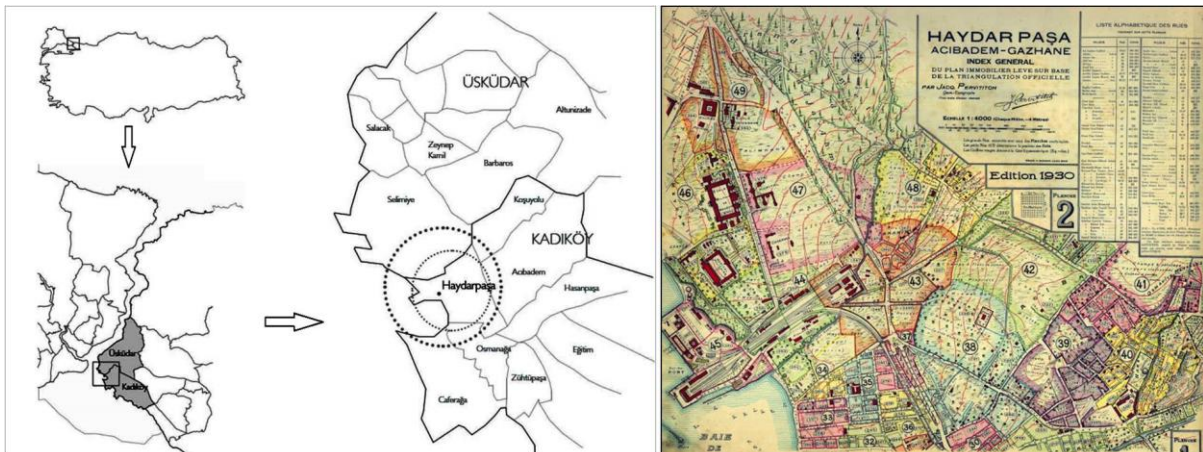


Figure 1. Haydarpasha Railhead and environs. 1930 Display on Pervititch Map [Url 1].

The first rails were tracked on 7 August 1871 from Izmit direction, on 24 August 1871 from Haydarpasha direction. By the way, while the construction of additional facilities on train routing

were started, a two-storey wooden masonry system in Neoclassic style Haydarpasha Railhead construction's foundation was laid by (so thought) Italian architect Giovanni Batista Barborini (1820-1891) in the area near English graveyard where the last foot of Today's Haydarpasha Bridge is situated (Serifoglu, 2016). In a short period such as six months, the first train started to travel toward Pendik station with its first siren on 22 September 1872 (Figure 2)

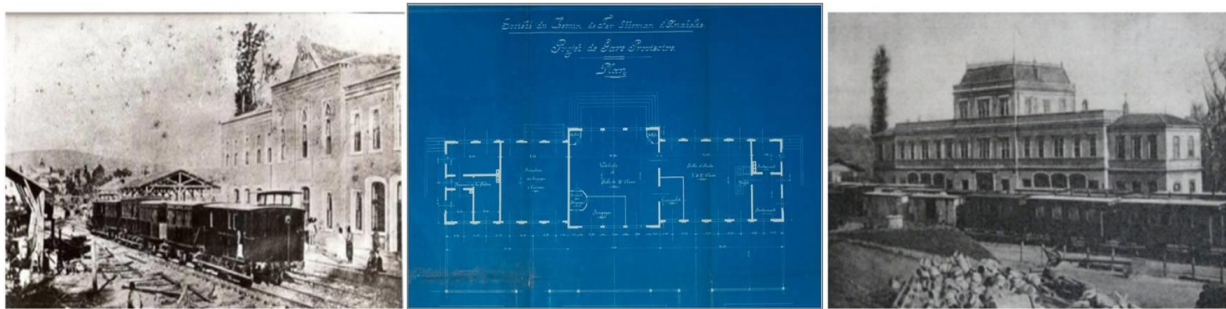


Figure 2. First Railhead of Haydarpasha 1870, its plan and display of 1890 after repairment [Url 2]

3. New Railhead Building

As days passed by, the old and small capacity railhead building become insufficient to meet the increasing heavy demand. A project competition was initiated for the construction of new railhead building, and architect Otto Ritter and engineer Helmut Cuno won the competition for the name of German Philipp Holzmann & Co. company as a result. The foundation of the magnificent building was prepared by riprap works and penetrating approximately 1700 piles to the stratum, and the first foundation was laid on 30 May 1906. Under the supervision of Otto Ritter and Helmut Cuno, the load-bearing façade system in Neorenaissance style was started to be constructed by yellow and green colored Bilecik Lefke and Hereke stones. By the completion of first passenger lounge and platforms at the building of which the opening was done with a spectacular ceremony, the works continued at its other parts. The whole of the building together with its facilities was completed on 4 November 1909 (Yildiran 2011).

4. Design Features of Haydarpasha Railhead

Haydarpasha Railhead consists of two arms in U form, long on North, short on South (Yildiran 1989), (Figure 3). It was positioned to front the jetties on north-west, south-east direction.



Figure 3. Haydarpasha Railhead environs building survey 2014 (Rail Road Archive)

The railhead building was constituted to 2525 square meter area at the beginning, and today it covers a 3836 square meter area together with its closed areas of today. Wooden piles that are isolated against the water were used on the foundation, and these piles were penetrated with steam hammer.

Its structure was done on the top of penetrate grate made of these penetrates. The load-bearing system is steel skeleton, and floor system is joist jack arch flooring (Kosebay, 2013).

According to the article published on Servet-i Funun journal on 25 June 1908 (13 June 1324), 2500 cubic meter Lefke stone, 13000 cubic meter concrete, 1140 ton iron, 520 cubic meter timber, 19000 meter hard wood and 6200 square meter slate roof covering was used (Figure 4)

25 June 1908 (13 June 1324)

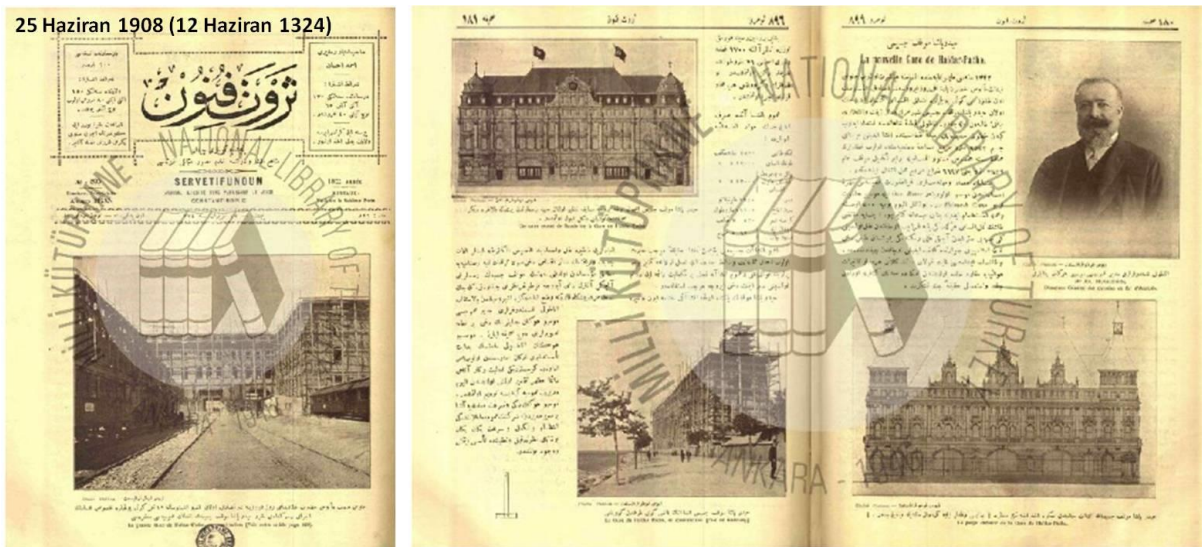


Figure 4. 25 June 1908 Material Information related to Haydarpasha on Servet-i Funun Journal [Url

4]

Beside the projects prepared by Otto Ritter and Helmut Cuno in 1906, there are also extensive environs building survey in 2014 on plans (Figure 6, 7, 10, 11, 13, 14). These first projects are being kept as blue print in State Railroads Archive (Figure 6).

The floorings of railhead building are made of pink granite, and load bearing and partitional walls are made of brick. The stones used are brought from Osmaneli, Hereke and Vezirhan. The construction is brick arch floor, tower floor at southeast corner is cross vault system.

Coating of Neorenaissance effected railhead building is made of yellow-green colored Lefke (Osmaneli) stone. North façade and façades facing the court are plastered in patches. The roof of the building was made of timber, and remade as steel structure in 1930 after 1917 explosion [Url 3].

Entrance to the passenger hall is by three doors with big basket-handle cove raised by stairs lying through the façade and being located on south façade. There are stained-glasses in the coves (Figure 5).



Figure 5. Exterior View of Haydarpasha Railhead Façade

4.1 Basement

Marinal façade of the basement is 71.21 m, long annex length 92.38 m, short annex length 55.29 m, wall thickness at front façade is 2.45 m, side annexes are 1.15 m, party walls are 0.75 m and 0.50 m. Along with the façade's right and left 4.30 m tower sides, there are passages among the mass stone rooms, and the basement is accessible via 1.10 m stairs from both annex ends through out of left tower and inside of right tower (Figure 6).

4.2 Ground Floor

At the building survey study dated 2014, the façade is seen as 71.09 m, right lateral façade southeast extension as 82.42 m, and short annex extension as 45.24 m dimensions (Figure 7). Ground floor's difference from the basement dimensions are sourced from interior regressions and wall contractions

at the ground floor. Wall thickness at the entrance façade is 1.80 m, court side wall thickness is 1.05 m. Fire escapes accessing till the roof from short and long annex heads are designed at floor plans. Entrance to waiting hall is via the main façade where the passengers enter into, and ticket offices are at left and right of the waiting hall. There are also Post Offices next to the Departure Directorship.

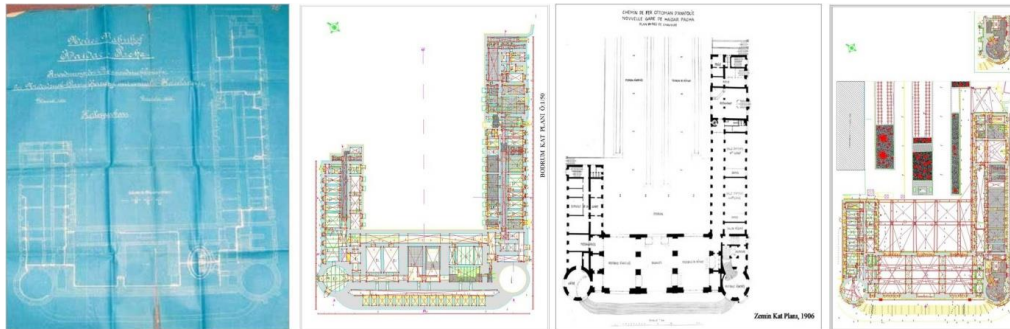


Figure 6. Building surveying plan dated 2014 prepared by Ritter and Cuno (Left) B. Floor (-3.30)

[Url 5]

Figure 7. Building surveying plan dated 2014 prepared by Ritter and Cuno (Right) G. Floor (+0.15) While Railhead Restaurant, Police Station, and Shops are at long annex, service stairs accessing until the roof are located at both annex ends.

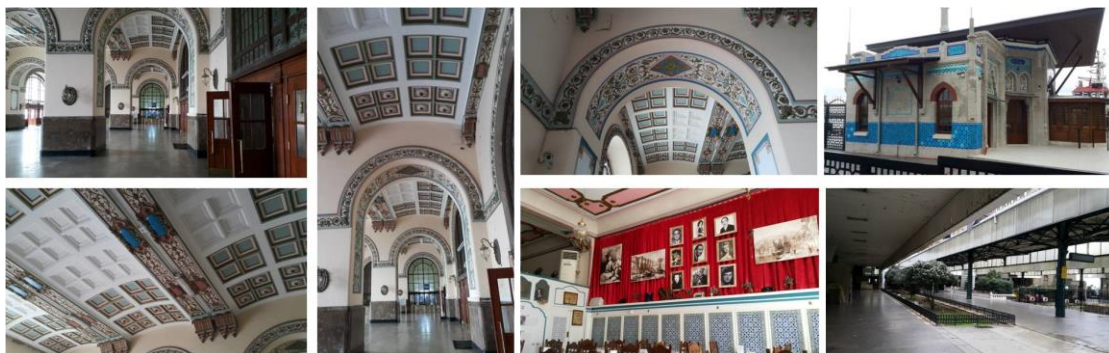


Figure 8. Ground Floor Railhead Entrance

While access until the management floors and General Directorate of TRSR (Turkish Republic State Railways) by an excellent entrance from the right tower, an elevator system providing access opportunity to the 3rd floor is situated at the middle of the stairs (Figure 9).

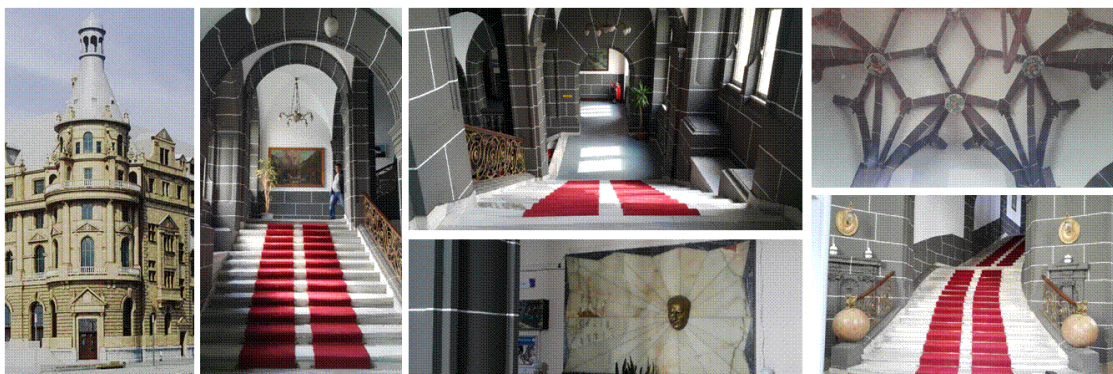


Figure 9. Right Tower Management Entrance

4.3 Mezzanine Floor

While +5.45 elevation is accessing to mezzanine and the right annex to office floors, the both annexes are also accessible via the stairs at the head. The ceilings are made of simple jack arch, and ceilings of some rooms have hand-drawn ornaments according to their functions (Figure 10).

4.4 First Floor

+8.94 above the waiting Hall, there are office, conference hall, and window wall office part is seen. At the north tower there are manager’s room and offices are on the short annex. This annex ends with bathroom and stairs. Southern tower is the general manager’s room. As it was mentioned before, there is a private bathroom in this room. As to the long annex, there are inspector’s office, other offices and bathroom. This aisle ends with the lodgement that is still in use today (Figure 11, 12).

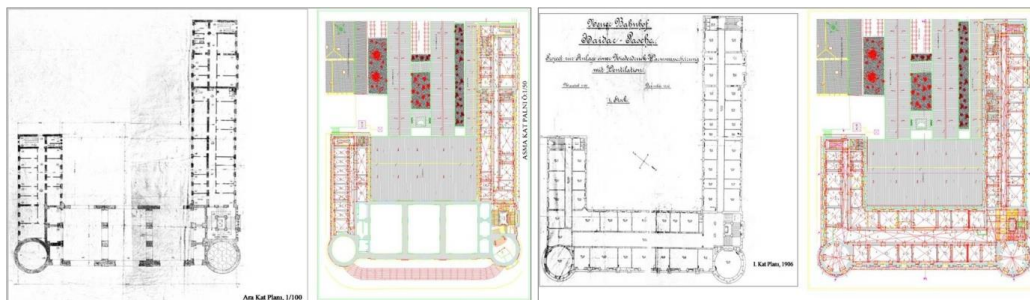


Figure 10. Building surveying plan dated 2014 and prepared by Ritter and Cuno (Left) Mezanine (+5.45)



Figure 12. First Floor Areas of Use

4.5 Second and Third Floors

All of the rooms lined on the right and left of the 2.20 m width aisle on the floor’s main façade and annexes are serving as office for State Railroads personnel.

Partition walls on the 14.00 m width annexes are taking the task of load bearing walls for the loads from roof to the basement. Parallel joists have made the jack arch system (Figure 13).

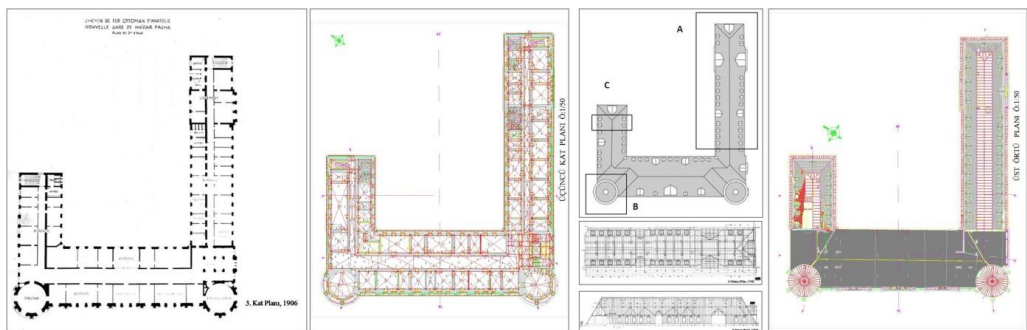


Figure 13. Building surveying plan dated 2014 and prepared by Ritter and Cuno (Left) 2nd Floor (+14.75), 3rd Floor (+14.55)

Figure 14. Building surveying plan dated 2014 and prepared by Ritter and Cuno (Right) Roof Plan (+23.70) (+0.15)

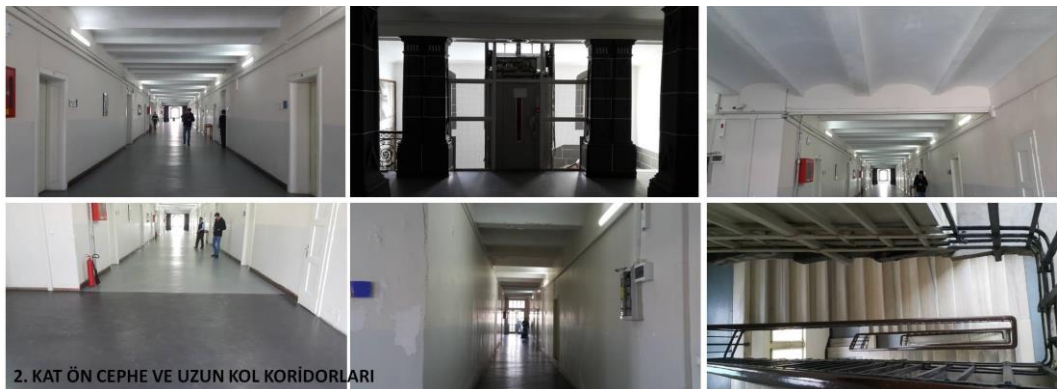


Figure 15. Floor Areas of Use

4.6 Roof

The whole of first railhead roof is constructed with timber system integrated to steel truss system. Architectural projects titled Bahnh of Haidar Pascha drawn in 1/50 scale put forward the detailed data about the timber roof system. Joists clenched with steel ring reinforcements at both towers. (Figure 14-16)

With these data, it is understood that all three arms of the roof is constructed with hipped roof. The numbers on clock tower are made of Arabic numbers, whilst they were changed after alphabet reform.

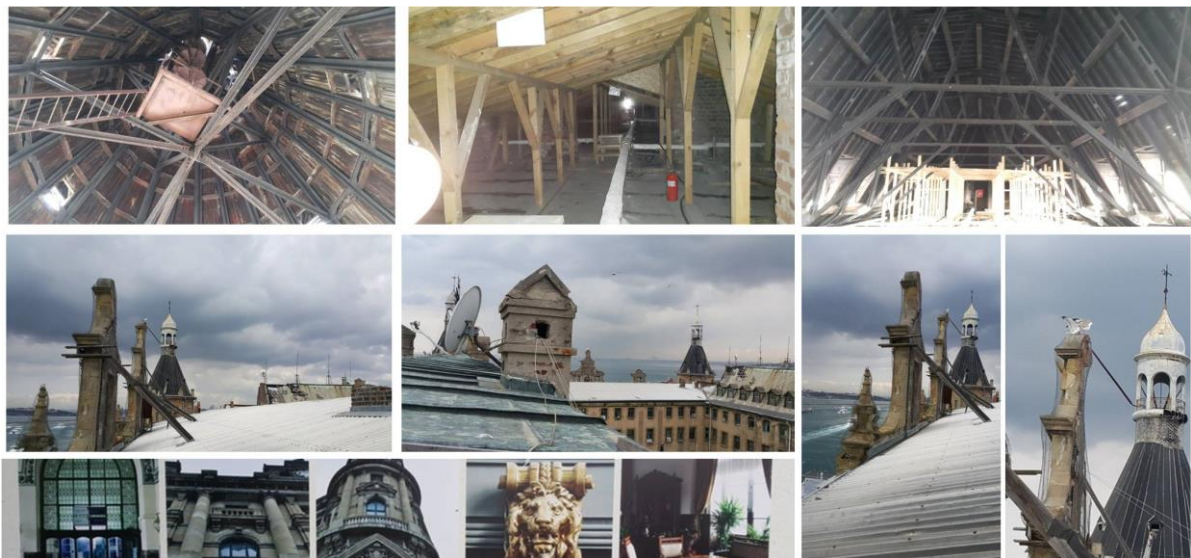


Figure 16. Precautions Taken on the Roof after the 2010 Fire

5. Haydarpasha Railhead and Its Environs' Urban Transformation Projects

1. In 2000 IMM (Istanbul Metropole Municipality) organized Kadikoy Square and Close Environs Urban Design Competition, the winner project kept the present functions of Haydarpasha Railhead, the project bringing the seaboard to the use of the public for recreational and cultural purposes avoiding the harbor is applied.
2. A project between 1989-2003 stopping the railroad function of Haydarpasha and using it as a hotel, hosting mall, trade center, and housing areas was created, but didn't find a response at the public opinion.
3. In 2004, together with IMM, TRSR has prepared under the seal of secrecy **“Zoning Plan for Kadikoy, Uskudar, Haydarpasha Railhead and Harbor Area” for the area including Haydarpasha and its 1 million square meter environment.** This project that was brought till the Protection Commission was cancelled with effective opposition of Haydarpasha Solidarity and decision of Protection Commission before it was brought to bidding phase.

4. After announcement of Haydarpasha Railhead and the area at its Environment as Urban and Historical Archeological Site, capitalist powers who want to have unearned income over that area have activated “1/5000 Scaled Master Development Protection Plan for Haydarpasha Railhead, Harbor and Back Space and Kadikoy Square and its Environs”. Haydarpasha Railhead and Kadikoy Square part of the plan is approved with the dated 21.03.2011 numbered 3200 decision of Istanbul Number 5 Cultural and Natural Heritage Preservation Board. Due to the cancellation of article 32/ of the law numbered 5335 by the Constitutional Court, transferring plan making authorization of TRSR to IMM was abolished, thus, BTS (United Transportation Syndicate) filed annulment action for the cancellation of above-mentioned commission decision at Administrative Jurisdiction [Url 1], (figure 17).

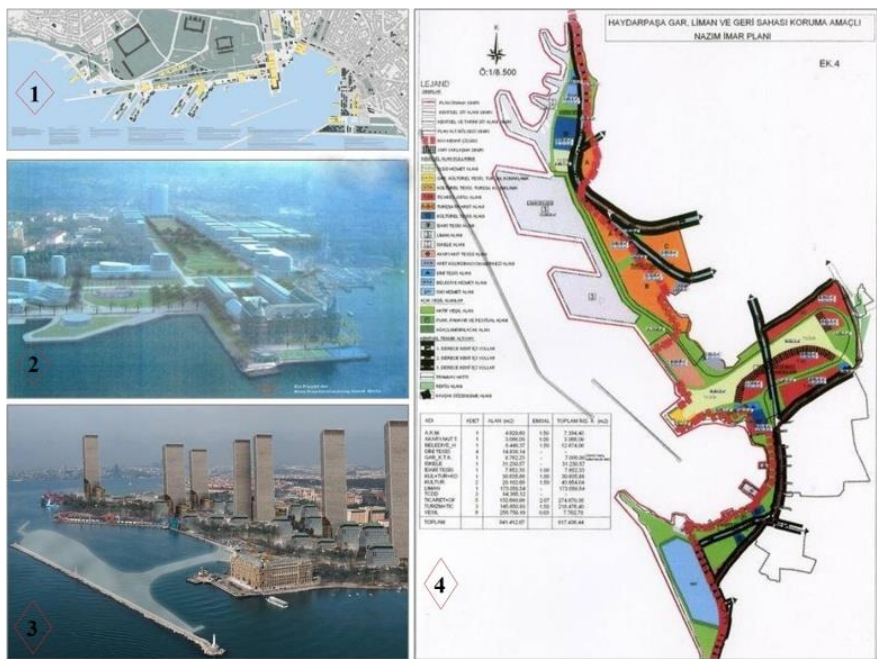


Figure 17. Haydarpasha Railhead and Its Environs' Urban Transformation Projects [Url 1]

6. Haydarpasha Railhead Registry Decisions

* With the dated 21.08.1997 numbered 4542 decision of Istanbul Number II Cultural and Natural Heritage Preservation Board, **Haydarpasha Railhead Building** is registered as **1st Group Cultural Assets that Must Be Protected**.

* With the dated 26.04.2006 numbered 85 of Istanbul Number V Cultural and Natural Heritage Preservation Board, **Haydarpasha Railhead and its Close Environs** is registered as **Urban and Historical Archeological Site**.

* Including **Haydarpasha Railhead in The List of Industry Tracks That Must Be Protected**, ICOMOS celebrated THE INTERNATIONAL DAY FOR MONUMENTS AND SITES at the panel named HAYDARPASHA-INDUSTRIAL HERITAGE AND PROTECTION it organized at ITU Faculty of Architecture Taskisla Building in the year 2006 on 18 April 2006 [Url 1].

7. Negativities Haydarpasha Railhead Lived

* Railhead building opened on 19 August 1908 lived its first disaster at the beginning of 1909, the building that fell into ruin was repaired and official opening was done on 4 November 1909.

* An ammunition which was prepared to be sent to Syria front exploded, the railhead, mostly the roof had substantial damages on 6 September 1917.

* British fighter aircrafts targeted the railhead parts on 4 July 1918 that were undamaged after the sabotage.

* In the midnight of 15 November 1979, the oil tanker named Independenta under Romania flag exploded because of allusion with another ship, lead stained-glasses were substantially damaged.

* On 28 November 2010, because of deflagration of isolation materials during the repairmen of railhead's roof, the roof heavily damaged. Using extreme amount of sea water have damaged the building structure in great extent (Figure 18)

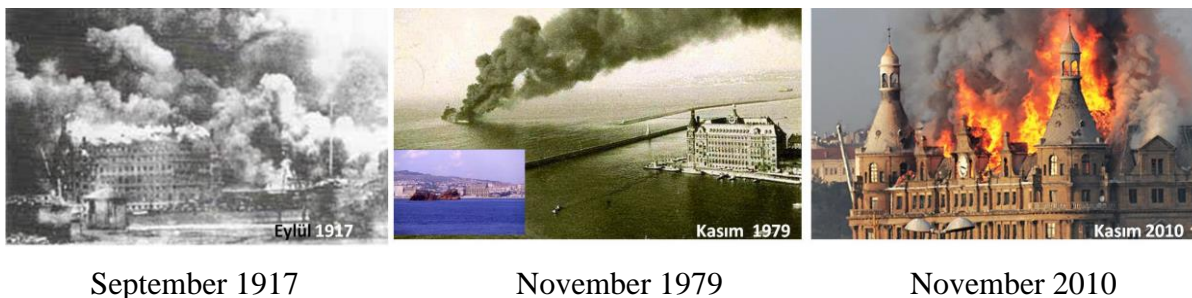


Figure 18. Explosions and Fires Haydarpasha Railhead Exposed to [URL 1]

8. 2014 Restoration of Haydarpasha Railhead

Haydarpasha Railhead of which its tender was put in 2014 and Extensive Building Surveying Projects were completed is going on the restoration process that was started in January 2016 by Delta Construction. An extensive program to know the building during the surveying process is run under the counseling of Prof. Dr. Vahit OKUMUS.

At the X-ray records taken on the basement, the 1700 number penetrates were visualized as approximately 100. The condition of all parts on the basement excluding left tower were all photographed, and material analyses were started to be taken under records. At this monumental

building that has no any other sample in Tukey, clearing of all extensions not belonging to the building was started to clean the marks of the all wrong restorations, and lead workshops were established on the platforms under the porch for true restoration.

First of all, together with ceiling ornaments in waiting hall, the restoration of two right and left towers were completed by attaching scaffolding to outer façade. To manage the roof repairment at left short annex, right long annex and marine façade and to prevent the building from the effect of climatic conditions, a new roof was constructed at a low elevation in the roof.

The existing steel skeleton was cleaned by sandblasting, cleaned completely of the dirt and rust, painted, and copper coated surface obtained completing the timber coatings. The stones on the clock tower at the marine side affected by climatic conditions were taken out by corroding and replaced with the new ones. Because the quarries of these stones from Bilecik region were closed, having the necessary permission from the related municipality, these quarries were opened to meet the need (Figure 19).



Figure 19. Haydarpasha Railhead Restoration Works after 2010 Fire

Together with the roof, the building façade was surrounded with the scaffold, and plaster repairment, moldings, cornice repairment continues. Despite what sort of function is going to be given to Haydarpasha Railhead is unascertained yet, pursuing the old function of the railhead is expected. During the repairments at Haydarpasha Bridge to integrate it to old Pendik line, they came across with historical Byzantium ruins belonging to early and late periods. While the renovation of the railhead is going on one side, the studies related to the historical findings are going on at the interval between Haydarpasha Bridge and the platforms (Figure 20).



Figure 20. Early and Late Period Byzantium Findings from the Soil during Haydarpasha Railhead
Reparation Works

In addition to these findings, city walls and antique harbor jetty were detected to enlighten, or even to rewrite the history of so called Land of Blinds (Chalcedon). Characteristic structure of 3.00 m width 4.00 m deep wall is made of face stone [Url6]. While the works to find out where the beginning and end of the wall are continues, documentation studies are also carried on. Since it is not possible to move this 300.000 square meter area that pullulates history, the necessity comes in sight to work up into an open air museum and redound it to the region (Figure 21)



Figure 21. Antique City Walls in 300 Decare Land [Url6]

9. Conclusion

For the last monumental structure of Ottoman State, Abdulhamit the Second has uttered this: *“I have railed that much of railroad for the country, the tip of the bars are at Haydarpasha. I have built harbor with huge buildings, albeit they are not apparent. Built such a building for me that, when my people look at it, “Once you get on here, you go till Mecca without getting out of the car” they shall say”*.

The last monumental structure of Ottoman State has managed to stand still till today within many misfortunes. While extensive maintenance works are continuing wishing the cleaning of the marks

left inside by several fires and people, the issue of what sort of function is going to be brought to the railhead is still a contention today.

An arrival and departure routing should be planned by reorganizing the line to enable accessing Sogutlucemesme Station on one side, Ayrikkemesme Station on the other side one each using a nostalgic car interrelating Haydarpasha Ferry Quay to make it live by protecting the existing circumstance of the Railhead. A 24 hour living opportunity for the region should be provided by rendering the findings belonging to Byzantium period found in the interval between Haydarpasha Bridge and Railhead in an in situ open air museum. Converting some parts of the railhead into activity areas such as exhibition, conference hall, library and book fair, is going to be the best road to follow, for sure.

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